



TOD District Zoning Code

Ronkonkoma Transit-Oriented Development (TOD) District

INTRODUCTION

This form-based code is established to implement the recommendations of the Ronkonkoma Hub Transit-Oriented Development Planning Study, dated March 2009. It creates a Regulating Plan that governs the public space, street types, and building forms for all future development within the Ronkonkoma Transit-Oriented Development (TOD) District (see Section 4). Land uses allowed by right are listed in Section 5. The Regulating Plan divides the Ronkonkoma TOD District into two “zones” – the Main Street Zone and the Neighborhood Zone (see Figure 1). Project proponents should first determine in which zone the proposed project is located and the street on which the project has frontage. This will determine the building form standards and public space standards apply to the proposed project.

Building form standards are established for each zone in the Regulating Plan that dictate the mix of uses, building height, parking, and location of the buildings (see Section 6). The TOD District also defines two different street types based upon the street right-of-way (see Section 7/Figure 2). Public space standards for the travel lanes, bicycle lanes, parking, median, and sidewalk are established for each street type as shown in Figure 2. Section 8 of the Code establishes the parking requirements, and Section 9 defines the design guidelines for parking areas, pedestrian and bicycle access, pedestrian spaces, facades, landscaping, lighting, site amenities, and signage.

SECTION 1. PURPOSE AND INTENT

The Regulating Plan (as defined in Section 2 hereof) and this Section of the Zoning Ordinance (Chapter 85 of the Town Code) shall govern land development activities within the Ronkonkoma TOD District. This Section establishes objectives, policies, and standards to promote orderly development and redevelopment within the Ronkonkoma TOD District for the purposes of encouraging high-density, mixed-use development, housing, retail, and office uses. The intent of this Section is to promote the development of Ronkonkoma TOD District in accordance with the Regulating Plan.

The purposes of this Section are:

1. Promote compact, mixed-use development in close proximity to the commuter rail station
2. Encourage development that supports transit
3. Encourage a diverse mix of business, commercial, office, residential, institutional, and entertainment uses for workers, visitors, and residents
4. Encourage pedestrian-friendly environment and pedestrian-oriented commercial enterprises and consumer services that do not rely on automobile traffic to bring consumers into the area
5. Encourage flexibility in site and architectural design

6. Maintain a consistently high level of design quality
7. Encourage building reuse and infill to create higher densities
8. Promote economic development opportunities

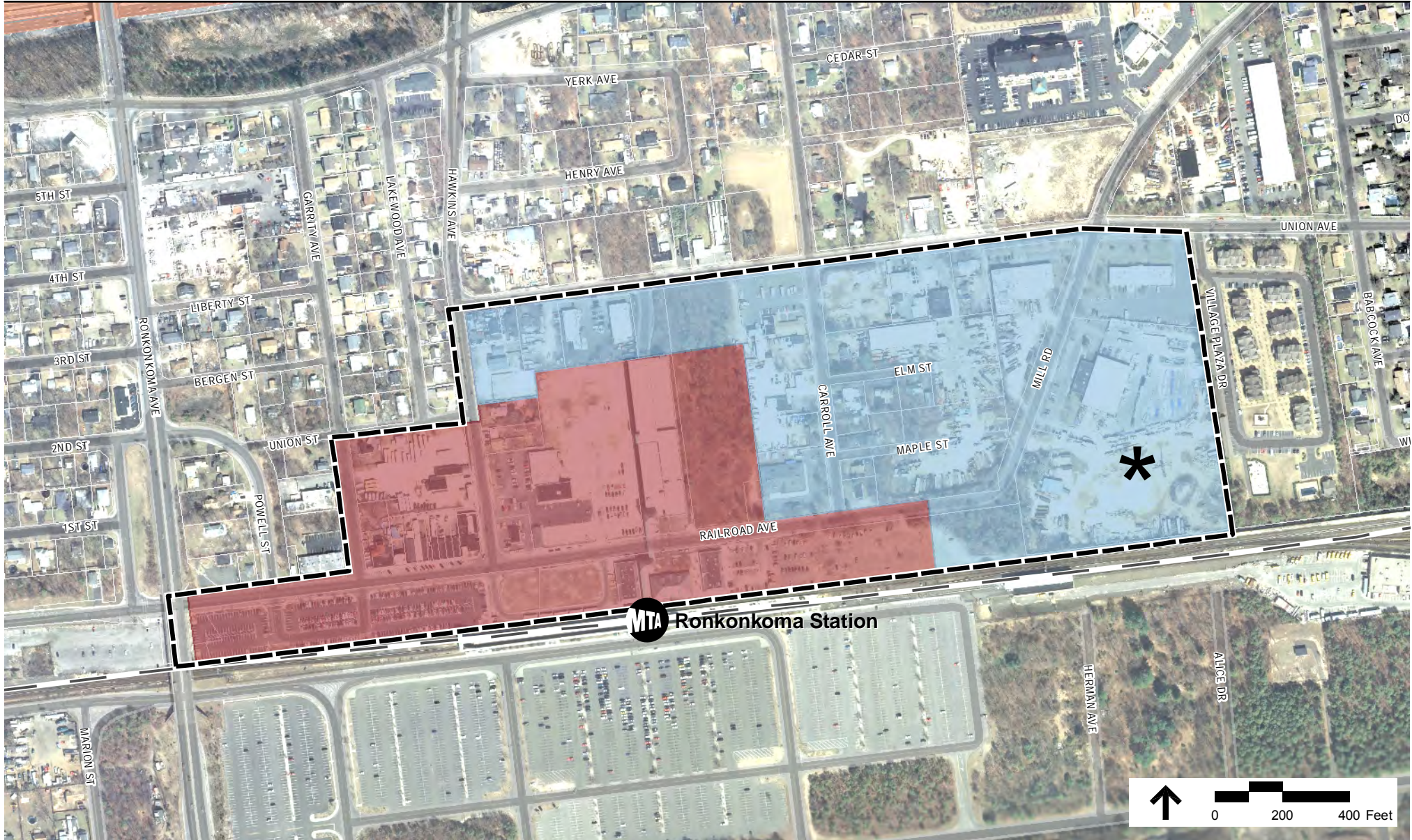
SECTION 2. REGULATING PLAN

The Ronkonkoma TOD District is delineated in Figure 1, the Regulating Plan. The Brookhaven Town Board adopted a Regulating Plan on [INSERT DATE]. The Regulating Plan is based upon the development principles that guide the development of the Ronkonkoma TOD District, as defined by the Ronkonkoma Hub Transit-Oriented Development Planning Study, dated March 2009. These principles are as follows:

1. Create a place rather than a series of unconnected projects.
2. TOD is created by market-driven factors rather than by the mere presence of a transit station. Retail development requires a good location, market, and design. Access to transit can strengthen the retail market.
3. There must be a mix of uses that includes retail, office, and housing, generally at a higher density. The land uses should be arranged to concentrate activity in proximity to the transit station. The land use mix falls into two categories—housing and employment uses generate transit ridership, and convenience retail and service uses support the riders and area residents.
4. Design guidelines, pedestrian amenities, and streetscape improvements are important considerations.
5. Parking standards must be carefully considered to include structured parking, common parking areas, and/or shared parking.
6. A mix of housing types and price levels is important.
7. Public-private partnerships can enhance TOD efforts.

The Regulating Plan and the Ronkonkoma Hub Transit-Oriented Development Planning Study, when read in concert with this Section, establish a comprehensive land use plan for development or redevelopment within the Ronkonkoma TOD District. In the review of all new development subject to these provisions, the Planning Board shall rely on the guidelines and objectives expressed in the Regulating Plan for the administration and enforcement of this Section.

As shown in the Regulating Plan, the sewage treatment plant falls within the Neighborhood Zone. If the site is not used for the sewage treatment plant, any proposed development of that site shall be completed in conformance with the building form standards for the Neighborhood Zone.



Data sources:
 Assessors Parcels, LIRR and Zoning – Suffolk County GIS
 Basemap – Town of Brookhaven, Long Island, NY
 Land Use – Field verification by VHB, Inc., 2010.

Legend

- Main Street Zone
- Neighborhood Zone

* Note that if this site is not used for the Sewage Treatment Plant (STP), it will be developed in conformance with the Neighborhood Zone.



Figure 1
 Regulating Plan

Ronkonkoma Hub
 Transit-Oriented Development

SECTION 3. DEFINITIONS

The following definitions apply only to the Ronkonkoma TOD District. All other terms shall be as defined in Section 85-1 of the Brookhaven Zoning Ordinance.

Awning: A roof-like covering, usually of canvas or metal, stretched upon a frame that is affixed to a building and used above or before any place as a shelter from rain or sun.

Building form standards: The part of the Code that establishes the basic parameters regulating the physical form of the built environment, including, but not limited to the envelope (in three dimensions), placement of certain permitted/required building elements, such as storefronts and balconies, and dimensional regulations such as setbacks and height.

Building height¹: The height of building shall be the vertical distance measured from the mean ground level of the established grade at the base of the building to the mean roof level (see Figure A).

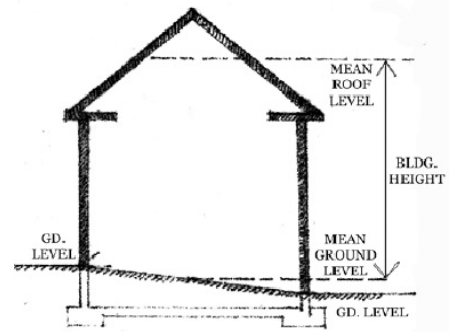


Figure A - Building Height

Building lot limit: A line delineating the outer edge of the buildable area, generally to the rear of a lot away from the required building line.

Façade: The portion of any exterior elevation on the building extending from grade to the top of the parapet, wall, or eaves and the entire width the building elevation (see Figure B).

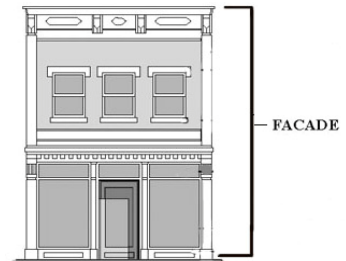


Figure B - Façade

Impervious surface: Any natural or manmade materials or structures on, above, or below the ground which do not allow surface water or precipitation to infiltrate the underlying soil, including, but not limited to building roofs, parking and driveway areas, sidewalks and paved recreation areas (see Figure C).

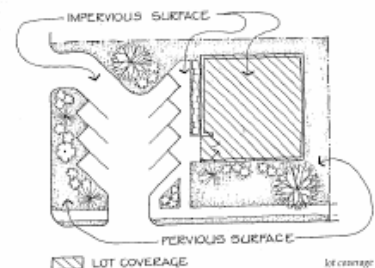


Figure C - Impervious Surface

¹ Note: This would apply only to Ronkonkoma TOD District since it measures height differently than the existing definition in the zoning ordinance.

Live-work dwelling units: A dwelling unit combining residential uses with work space set aside for commercial or limited industrial uses permitted in the TOD District. The work space shall be accessible from the living area, reserved for, and regularly used by, one or more persons residing therein.

Mixed-use development: The development of a lot of land or building or structure with two or more different uses, including but not limited to, residential, office, and retail in a compact urban form.

Pedestrian walkway: A public or private right-of-way across or within a block to be used by pedestrians.

Regulating plan: The implementing plans for the development of the district pursuant to this Chapter. The regulating plan establishes the building form standards and street types and provides specific information for the disposition of each building site. It also shows how each site relates to the overall district and the surrounding neighborhoods.

Required Building Line (RBL): A line or plane indicated on the regulating plan, defining the street frontage that extends vertically and generally parallel to the street, at which the building shall be placed. The facade shall occur on the required building line - this is a requirement, not a permissive minimum. The minimum length and height of frontage that is required at the RBL is shown on the appropriate building form standard.

Street-space: All space between fronting streets, plazas, pedestrian paths, sidewalks, civic spaces, etc., but not including parking entries and driveways.

Street wall: A masonry wall set back from the required building line which assists in the definition of the street-space in the absence of a building, or part of the building nearest to the street line.

SECTION 4. ZONING MAP

The boundaries of the Ronkonkoma TOD District is hereby established as shown on the map entitled Brookhaven, NY Building Zone Map, as or hereafter amended, which map is made a part of this Chapter.

SECTION 5. PERMITTED USES

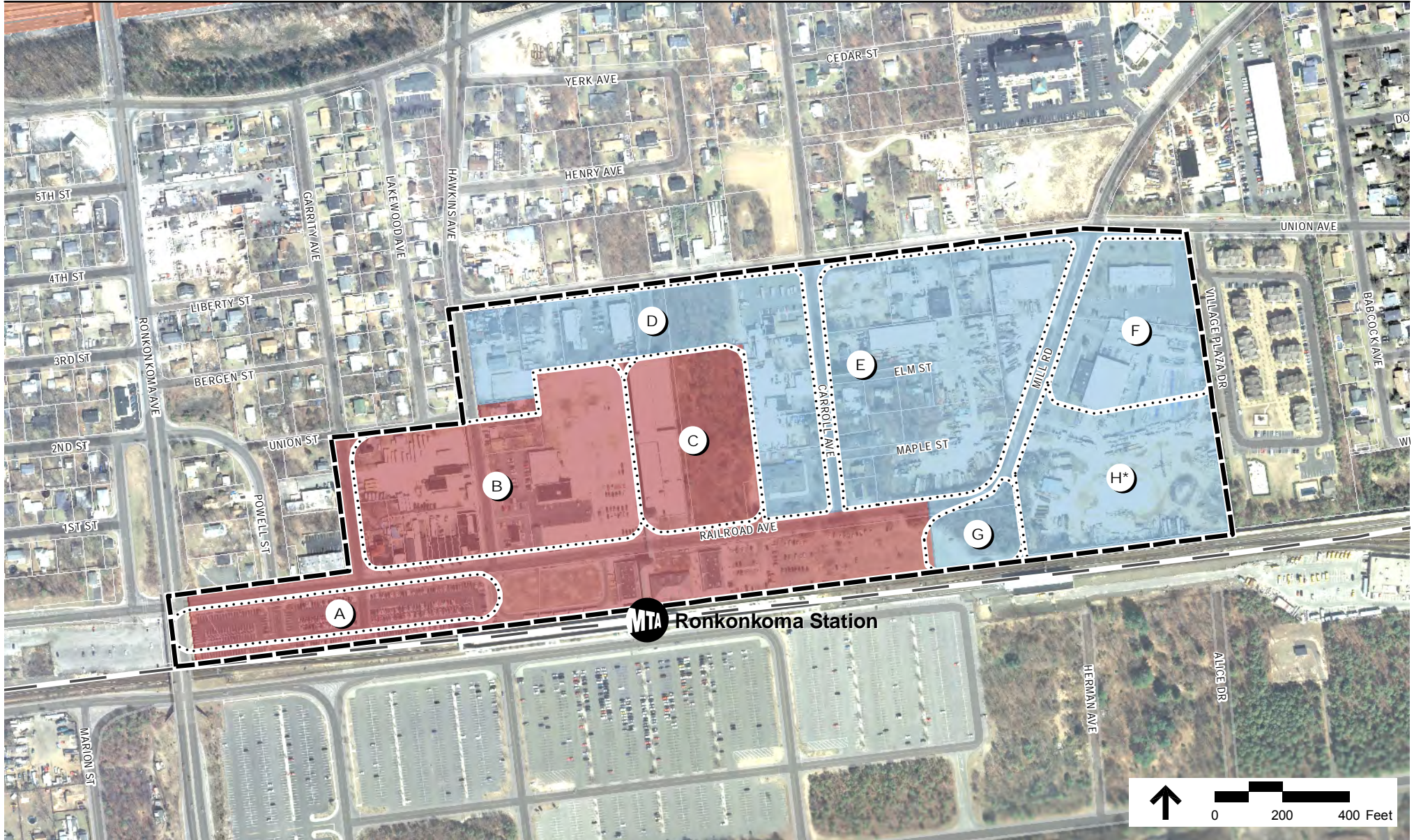
1. Permitted uses
 - a. Townhouses
 - b. Duplexes
 - c. Multi-family housing
 - d. Live/work dwelling units
 - e. Mixed use developments
 - f. Professional offices such as those of doctors, lawyers, architects and engineers

- g. Retail businesses such as, but not limited to grocery, drug, apparel, variety, furniture, or sporting goods store
 - h. Restaurants and eating and drinking establishments where most food and drink is intended to be consumed on the premises at tables, counters, or bars
 - i. Personal services such as barber shops, beauty salons, laundry and dry cleaning establishments
 - j. Health club
 - k. Business services such as banks and other financial institutions, real estate and insurance offices
 - l. Accessory buildings and uses
 - m. Child care facilities
 - n. Museum and similar cultural facility of a non-commercial nature
 - o. Home occupations
2. No new industrial uses shall be permitted in the Ronkonkoma TOD District; however, industrial uses existing in the District at the time of adoption of this Section shall be allowed to continue to exist.
3. Any pre-existing nonconforming uses or structures shall be governed by Section 85-372 of the Brookhaven Zoning Ordinance.

SECTION 6. BUILDING FORM STANDARDS

1. Purpose
- a. The goal of the building form standards (BFS) is to create a vital, and coherent public realm that defines the physical form of the built environment. The intent of these form standards is to shape the specific physical and functional character of the district. The form controls on building frontages work together to frame the street-space while allowing greater functional and operational freedom behind their facades. The building form standards aim for the minimum level of control necessary to meet this intent.
 - b. The BFS set the basic parameters governing building construction, including the building envelope (in three dimensions) and certain required or permitted functional elements, such as fenestration (windows and doors), stoops, balconies, front porches, and street walls.
 - c. The BFS establish the rules for development and redevelopment on private lots.
 - d. The regulating plan identifies the building form standard for all private building sites within the district.

	Main Street Zone	Neighborhood Zone
Purpose	To enhance the vibrant, pedestrian-oriented character of Railroad Ave. by creating a “main street” pedestrian friendly commercial area	To encourage a variety of housing types to enable a diversity of residents in a manner that yields medium to high densities that will help support the transit facilities and commercial uses in the surrounding zones
Use Mix	Vertical mixed use with retail on the ground floor and residential or commercial above	Primarily residential area that provides the core residences in medium to high density building types and allows for a limited amount of ground floor commercial use and live/work units
Heights	3 – 5 stories	2 – 4 stories
Parking	Building over grade level parking and/or parking lot	Building over grade level parking or parking lot
Location (see Figure 2)	A, B, C	D, E, F, G, H



Data sources:
 Assessors Parcels, LIRR and Zoning – Suffolk County GIS
 Basemap – Town of Brookhaven, Long Island, NY
 Land Use – Field verification by VHB, Inc., 2010.

Legend

- Main Street Zone
- Neighborhood Zone
- Building Form Location
- * If Sewage Treatment Plant (STP) is not built in this location.



Figure 2
 Building Form Locations
Ronkonkoma Hub
 Transit-Oriented Development

2. General Provisions

The Ronkonkoma TOD District code includes two building form standards – Main Street and Neighborhood. The following standards apply to all building forms, unless expressly stated otherwise within an individual BFS.

3. Transitions

When the BFS designation shown on the regulating plan changes along a property's required building line (RBL), the property owner has the option—for that property's street frontage only—of applying either BFS for a maximum additional distance of fifty (50) feet in either direction along the RBL.

4. Building Height

The height of all buildings is measured in stories, with an ultimate limit in feet, measured from the mean ground level of the established grade at the base of the building to the mean roof level (see definition of building height). The maximum building height shall not exceed five (5) stories or sixty-five (65) feet for areas A, B, and C and four (4) stories or fifty (50) feet for areas D, E, F, G, and H (see Figure 2). Chimneys, mechanical equipment, antennas, skylights, bulkheads, tanks, or solar panels shall not be considered part of the height of the building if they do not extend more than four (4) feet above the specific height limit, but shall be shielded from view from the street level.

5. Where a site abuts an existing single family district, a landscaped berm, at least four (4) to six (6) feet in height, shall be constructed.

6. Siting of buildings

- a. Within thirty (30) feet of a block corner, the building facade shall be built to the RBL, unless otherwise specified in the BFS.
- b. A street wall shall be required along any RBL frontage that is not otherwise occupied by a building, unless otherwise designated in the BFS or Regulating Plan. A required street wall shall be located not more than eight (8) inches behind the RBL.
- c. The RBL, designated on regulating plan as an absolute line, incorporates an offset area (or depth) of twenty-four (24) inches into the buildable area allowing for jogs, facade articulation, etc.
- d. Unless otherwise designated herein, where the façade is placed within that twenty-four (24) inch depth, it shall be considered to be "built-to" the RBL.
- e. No part of any building may be located outside of the buildable area except overhanging eaves, awnings, stoops, bay windows, handicapped ramps, signage or balconies, or other architectural features as approved by the Planning Board.
- f. No part of any building may be located outside of any designated lot building limit.
- g. There are no side yard setbacks. No part of any building, including its architectural features, shall extend beyond the side or rear property lines.

- h. Curb cuts or driveways shall be located at least seventy-five (75) feet away from any block corner or alley access point on the same block face.

SECTION 7. PUBLIC SPACE STANDARDS

1. Street type specifications

The street type specifications provide typical configurations for street-spaces within the Ronkonkoma TOD District. The plans and sections specify vehicular travel lane widths, sidewalks, tree planting areas, bicycle lanes, medians, and on-street parking configurations.

2. Street type designations

These are the permitted types and configurations permitted within the Ronkonkoma TOD District (see Figure 3). The numbers refer to dimensions within the street-space.

- a. Street type 1 – Railroad Ave., Garrity Ave., and Hawkins Ave. (see Figure 4)
- b. Street type 2 – Union Ave., Union St., Mill Rd., and Carroll Ave. (see Figure 5)

3. General design principles for streets

- a. Streets must balance the needs of all modes of transportation (auto, transit, bicycle and pedestrian) to maximize mobility, safety, and convenience for all residents and users. Specific street elements such as road width, bicycle lanes, sidewalks, etc. can be found in Figures 4 and 5.
- b. Scale is a primary design consideration for street design elements (from signage to crossing distances).
- c. An interconnected street network allows traffic capacity to be diffused and maintained across numerous streets.
- d. Emergency vehicle access must be maintained, but with an interconnected street network, there will always be at least two routes of access to any lot or parcel.
- e. Overall function, comfort, safety, and aesthetics of a street are more important than efficiency alone.
- f. In a pedestrian-oriented area, non-vehicular traffic should be provided with every practical advantage so long as safety is not adversely affected.



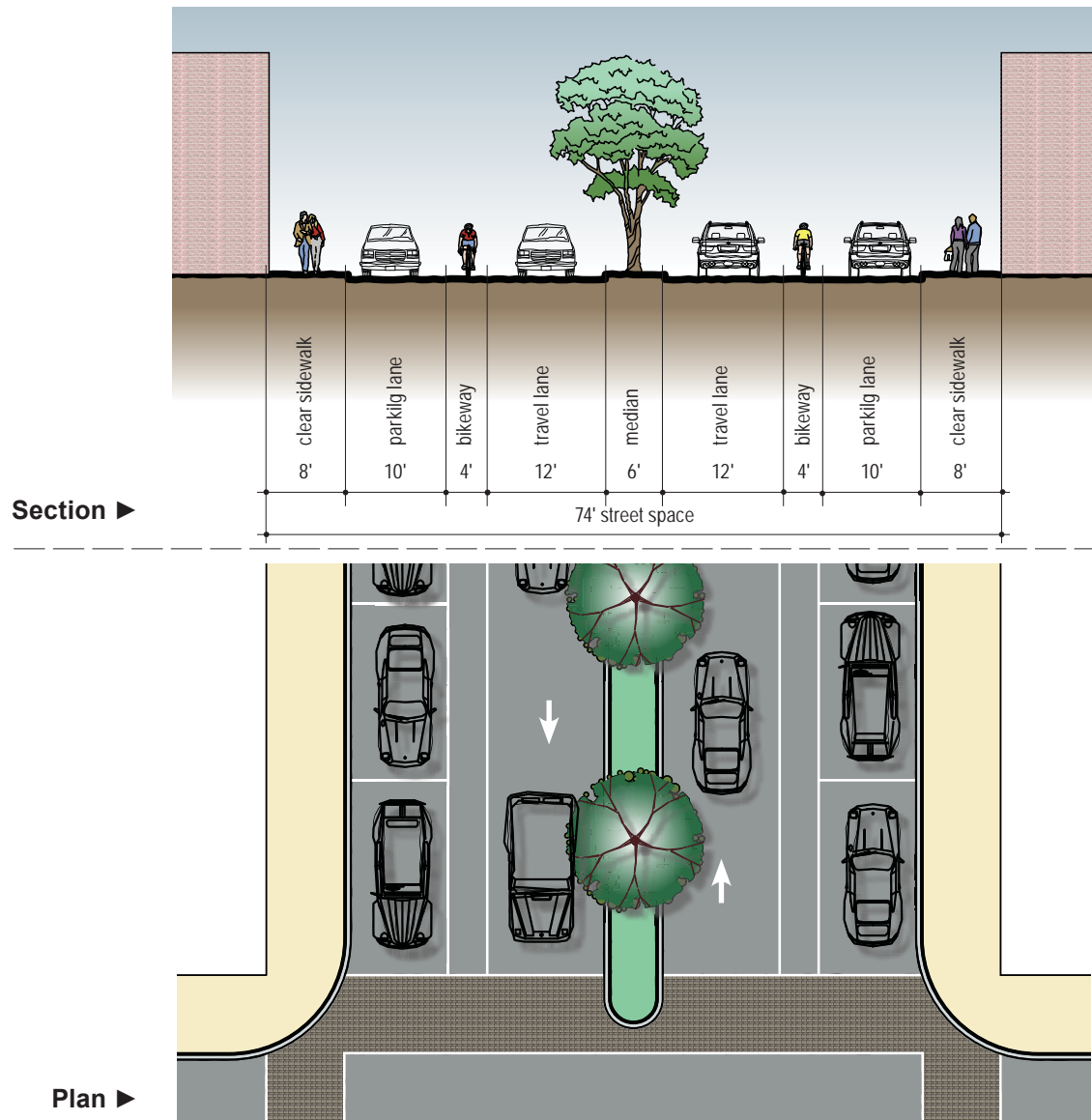
Data sources:
Assessors Parcels, LRR and Zoning – Suffolk County GIS
Basemap – Town of Brookhaven, Long Island, NY
Land Use – Field verification by VHB, Inc., 2010.

- Legend**
- Street Type 1
 - Street Type 2



Figure 3
Street Types

Ronkonkoma Hub
Transit-Oriented Development



Street Elements

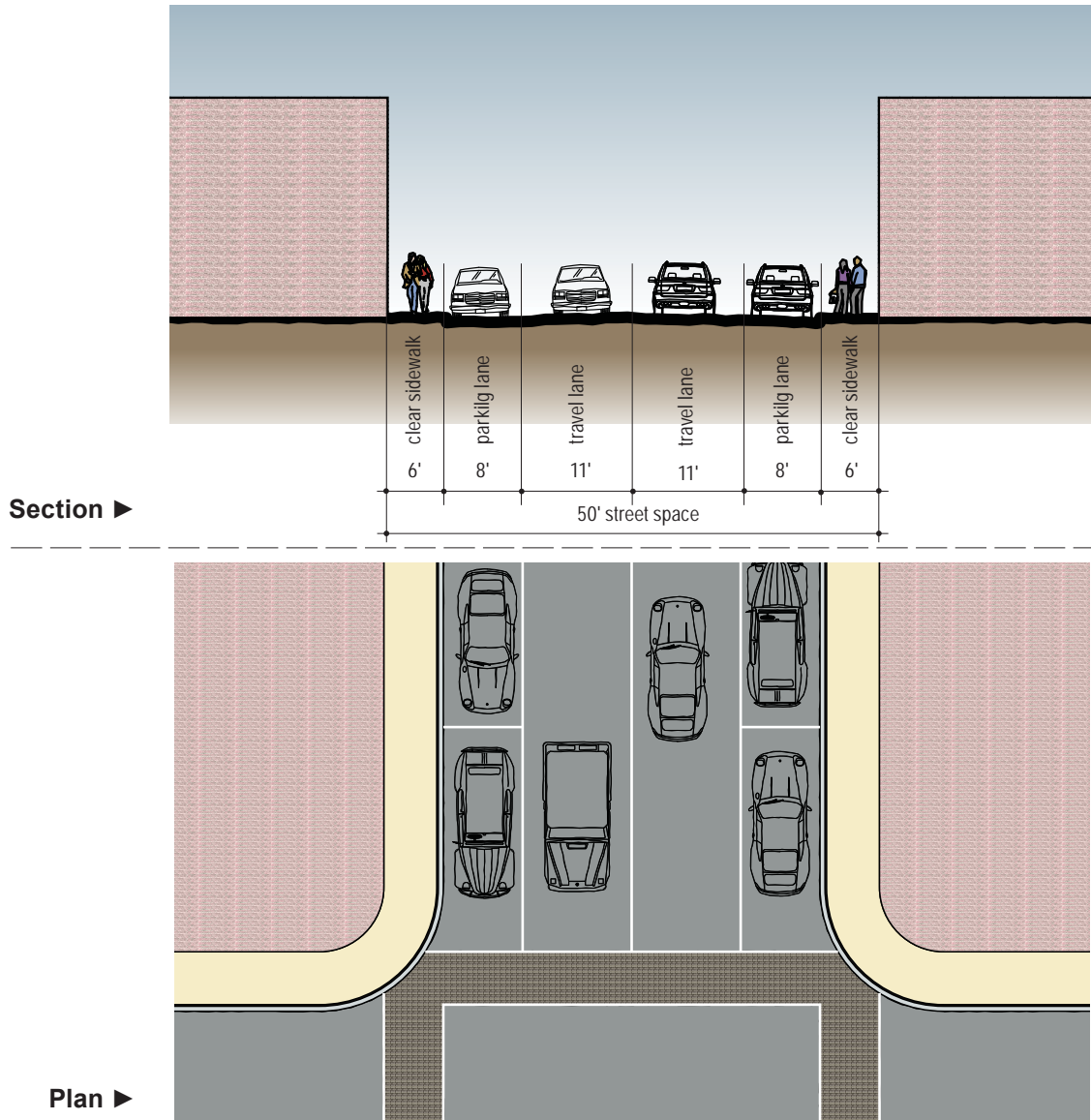
GENERAL	
Right of Way	74'
Curb-to-Curb Width	58'
TRAVEL LANE	
Number of Lanes	2
Width	12'
BICYCLE LANES	
Number of Lanes	2
Width	4'
PARKING LANES	
Number of Lanes	2
Width	10'
MEDIAN (Y/N)	
Width	Variable – up to 6'
SIDEWALK (Y/N)	
Both Sides (Y/N)	Y
Width	8'



Figure 4
Street Design of Railroad, Garry and Hawkins Ave (Street Type 1)

Ronkonkoma Hub
Transit-Oriented Development

Prepared for the Town of Brookhaven, June 2010



Street Elements

GENERAL	
Right of Way	50'
Curb-to-Curb Width	38'
TRAVEL LANE	
Number of Lanes	2
Width	11'
BICYCLE LANES	
Number of Lanes	0
Width	n/a
PARKING LANES	
Number of Lanes	2
Width	8'
MEDIAN (Y/N)	
Width	n/a
SIDEWALK	
Both Sides (Y/N)	Y
Width	6'



Figure 5
Street Design of Mill Rd, Union Street,
Union Ave and Carroll Ave (Street Type 2)

Ronkonkoma Hub
Transit-Oriented Development

Prepared for the Town of Brookhaven, June 2010

4. Streetscape Standards

- a. All plant material between the RBL and the back-of-curb shall be irrigated and maintained by the adjoining property owner.
- b. All turf grass shall be sodded at installation. Vegetative groundcovers may be used in place of turf grass.
- c. In addition to the lot, the owner must maintain the following areas:
 - i. The portion of the street-space between their RBL and the back of the curb.
 - ii. The portion of the alley or common drive between the lot line and the edge of the alley pavement.

5. Mechanical and electrical equipment including, but not limited to, air compressors, pumps, exterior water heaters, water softeners, private garbage cans (not including public sidewalk waste bins), and storage tanks may not be stored or located within any street-space. Water pumps for public fountains or irrigation that are not visible are excluded from this prohibition.

6. Street trees

The following standards apply for the planting of street trees on Street Type 1. Street trees are not required for Street Type 2.

- a. Each street must have street trees planted along the street tree alignment line (generally 3 to 3 ½ feet from the back of the curb) at an average spacing not greater than thirty (30) feet on center (calculated per block face). Where necessary, spacing allowances may be made to accommodate Americans with Disabilities Act (ADA) requirements, curb cuts, fire hydrants and other infrastructure elements. See Section 85-50.1 of the Brookhaven Zoning Ordinance for a list of approved street trees.
- b. Required street tree planting minimum specifications are as follows:
 - i. Planting area shall be at grade or not greater than six (6) inches in height above or below the sidewalk
 - ii. Any unpaved ground area shall be planted with groundcover or flowering vegetation not to exceed twelve (12) inches in height.
 - iii. Street trees at planting shall be at least three (3) inches in diameter (at DBH) and at least fifteen (15) feet in overall height.
 - iv. Must be “limbed up” as they gain appropriate maturity so as to not interfere with pedestrian or truck travel (minimum 7 feet clear over the sidewalk and 14 feet over the travel lanes of the street) and to maintain visibility.

7. Other streetscape elements

- a. Street lights shall be installed on both sides of streets, aligned with the street trees, at intervals of not more than eighty (80) feet, measured parallel to the street.
- b. Street lights shall be between nine (9) and sixteen (16) feet above ground in height.

- c. At the time of development, the developer is required to install street lights and sidewalks, as illustrated in Street Type Specifications, on the side of the street being developed.
 - d. Sidewalks shall be constructed to meet all Town (and ADA) specifications.
 - e. Street furniture shall be located along the street tree alignment line for Street Type 1, but in no case shall be allowed to obstruct the clear walkway.
8. Public space standards
- a. The public space standards apply to new development as well as the reconstruction of existing streets and other public (and publicly accessible) spaces within the Ronkonkoma TOD District.
 - b. The urban space standards establish the rules and standards for the public realm (especially streets and sidewalks). This includes property frontages and building facades. They establish an environment that encourages and facilitates pedestrian activity—“walkable” streets that are comfortable, efficient, safe, and interesting.
 - c. In order to accommodate the establishment of common space open to the public such as outdoor restaurant seating, and to meet ADA sidewalk requirements, a building may be set back up to fifteen (15) feet from the RBL so that the common space is placed behind the RBL thereby allowing the minimum sidewalk requirements for each Street Type to be met.
 - d. Squares, civic greens, plazas and pedestrian pathways
 - i. Squares, civic greens and plazas should be situated at prominent locations within each mixed-use center. The plants and trees of squares and civic greens provide a landscape and civic architecture that complement the surrounding private building architecture.
 - ii. Pervious paving materials (to allow oxygen for tree roots and absorb stormwater runoff) are encouraged in both squares and civic greens, and the percentage of impervious paving material is limited.
 - iii. Street trees should generally be of a different species than those on the connecting streets. At least sixty (60) percent of the perimeter fronting rights-of-way shall be surrounded by street trees.
 - iv. Asphalt is prohibited within a square, civic green, or quadrangle.
9. Pedestrian pathways
- The area within a pedestrian pathway shall be a public access easement or public right of way. The easement width for these pathways shall not be less than ten (10) feet with a paved walkway not less than six (6) feet wide and should provide an unobstructed view straight through its entire length.

SECTION 8. PARKING REQUIREMENTS

1. Base Parking Requirements

Land Use	Minimum Spaces Required
Residential	1.20 spaces per dwelling unit
Restaurants	.33 spaces per seat
Commercial	2.65 spaces per 1,000 sq. ft. of building area
Live/Work Space	1 space per 1,000 sq. ft.
Office	2.86 spaces per 1,000 sq. ft. of floor area

On-street parking spaces shall count towards parking requirements.

2. Parking areas

The following guidelines are included to ensure that off-street parking areas are constructed in accordance with the district's desired design character and the provisions of this Section.

- a. Parking lots for new construction shall be located to the side and rear of the lot. Parking is prohibited within the front yard. Parking lots that abut public rights of way or grade parking under the building shall be screened with one or a combination of the following:
 - i. A low wall made of concrete, masonry or other suitable material not exceeding a height of three (3) feet.
 - ii. Raised planters planted with a minimum of eighty (80) percent evergreen shrubs not to exceed a total height of five (5) feet (including planter).
 - iii. Landscaping consisting of a mix of trees and shrubs provided that eighty (80) percent of the shrub plantings are evergreen.
- b. All off-street parking shall be located behind buildings that face on a street or public open space, and be accessed by alley or short driveway between buildings.
- c. Pedestrian access shall be provided between structured parking and the public street.
- d. Walls, fencing and architectural details shall complement the materials of adjacent architectural styles and the character of the development as a whole.
- e. Where walls are provided, planting areas shall be a minimum width of 4 feet and should be located adjacent to the public right of way.
- f. Where possible, parking areas shall be interconnected in a manner that allows the unobstructed flow of pedestrians between uses and parking areas.
- g. In large parking lots (7 or more spaces) provision for bicycle racks shall be provided in locations that are safely segregated from automobile traffic and parking.

- h. Shared parking. Shared parking is the approved use of the same off-street parking spaces for two (2) or more uses where peak parking demand of the different uses occurs at different times of the day, or, where various uses are visited without moving the automobile; and, where the division of parking spaces is a net decrease from the combined total of each use's individual off-street parking requirements, if required separately.
 - i. Requirements and Criteria. Shared parking arrangements are subject to review and approval by the Planning Board subject to the following requirements and criteria:
 - a) Submission of a reciprocal agreement executed by the owners and operators of the different sources or uses ensuring the long-term joint use of such shared parking, and defining the terms upon which the parking is shared;
 - b) Submission of the following information shall be submitted to the Planning Board:
 - 1. the hours of operation and parking demand for each use;
 - 2. the hours of peak demand for parking;
 - 3. a description of the character of the land use and the parking patterns of adjacent uses;
 - 4. an estimate of the anticipated turnover in parking space use over a 24 hour period of time;
 - 5. a site plan showing all proposed parking spaces, including the shared use spaces in the lot and the walking distance to the uses sharing the lot; and
 - 6. Any other information concerning parking deemed necessary by the Planning Board to render a decision.
 - ii. Decision. A determination shall be made by the Planning Board that:
 - a) The shared parking is no more than four hundred (400) feet from each use sharing the parking facility;
 - b) The hours of operation and peak demand of the uses involved shall not conflict; and
 - c) The shared parking will provide an adequate number of spaces for the applicable uses.
 - iii. In the event that the conditions for shared parking change, or if the shared parking arrangement is discontinued, the applicant shall notify the Planning Board within ten (10) days. The Town Board shall then require the applicant to meet the applicable parking requirements found in Section 8.1 of this TOD District code.

SECTION 9. DESIGN GUIDELINES

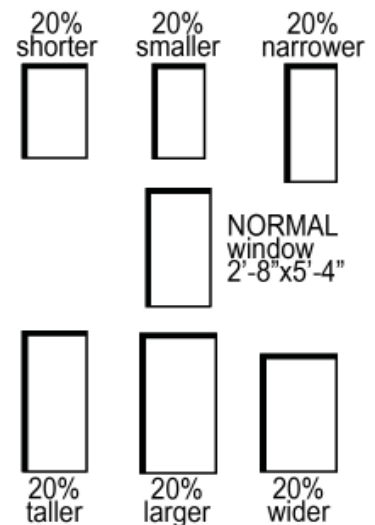
1. Pedestrian and Bicycle Access

- a. Provision for safe and convenient pedestrian access shall be incorporated into plans for new construction of buildings and parking areas and should be designed in concert with landscaping plans noted below. Site plans in the Ronkonkoma TOD District should provide for continuity from sidewalks in public streets to all pedestrian entrances on the site, and walkability should be given primary importance over road speed and other access criteria. New construction should improve pedestrian access to buildings, sidewalks and parking areas and should be completed with consideration of pedestrian safety, handicapped access and visual quality. Where appropriate, applicants are encouraged to provide pedestrian and/or bicycle paths connecting the site with abutting areas in order to promote pedestrian and bicycle circulation and safety. When parking is located in the rear, pedestrian access via a pedestrian-oriented alley or walkway through to the primary street is encouraged.
- b. On-street bicycle parking shall be provided forward of the dooryard area. The racks shall be located in alignment with the street trees and shall not block the clear walkway. The “U” rack is recommended as the standard rack.

2. Facades

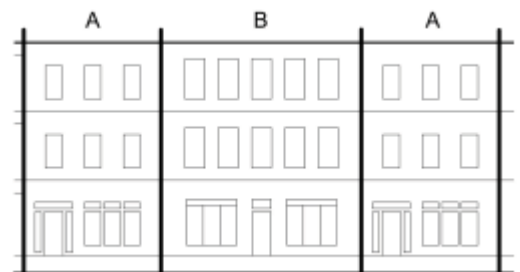
Facades shall have the following characteristics.

- a. Clearly different ground story facade composition (both framing materials and fenestration proportions).
- b. Different window proportions (minimum difference 20 percent). *See illustration below.*
- c. Different facade composition (clearly different ‘bay’ rhythm, e.g. ‘ABA’ –‘ABBA’ – ‘BAAB’ ‘ABCBA’). *See illustration below.*
- d. Different wall material (clearly different external wall material – color changes do not suffice).
- e. Each facade composition shall include a functioning street entry door at intervals not exceeding 75 feet.



3. Pedestrian Spaces and Comfort

For the purpose of providing a pedestrian-friendly environment in the Ronkonkoma TOD District, new and redeveloped buildings should provide for outdoor seating areas, scaled to the size and demands of the



proposed use, where feasible. For example, a large, multi-story project should provide a patio or small plaza area located near the front entry with multiple benches and landscaping. A mixed-use project with ground floor retail such as a restaurant may provide an area for outdoor dining which extends the indoor dining space for seasonal use. A ground floor use may provide a sidewalk bench where there is sufficient width.

Such pedestrian areas are best located when they take advantage of southern exposure and provide space that affords visual connectivity but is setback from major pedestrian flow and vehicular ways and is appropriate to the location.

Outdoor sales and display areas should be well organized and located so as not to impede pedestrian circulation if located on a public walk or way.

The following guidelines should be considered in the design and location of pedestrian spaces:

- a. Flexible design to allow for flexible use
 - b. Buffering from major vehicular areas such as parking lots or main traffic ways
 - c. Lighting for nighttime comfort and safety
 - d. Appropriate street furnishings...i.e. benches, trash receptacles, planters, etc.
 - e. A focal element, where appropriate, such as a water feature, special landscape feature or public art installation
 - f. Decorative paving and seasonal planting
 - g. South facing locations
 - h. Visual connectivity, especially to important views such as an historic structure
 - i. Appropriately scaled to the development
 - j. Provide for continuity of pedestrian sidewalks
4. Landscaping. Landscaping shall be incorporated into new and redeveloped properties in such a way as to create visual relief and interest, provide shade for pedestrian areas and to screen parking and loading areas. Landscape plans shall be prepared by a registered landscape architect or may be accepted, where at the Planning Board's discretion the plans submitted are found to be consistent with the intent of this regulation and meet the specific guidelines as set forth herein. Landscape plans shall show the location, type, and size of all proposed plantings as well as enough of the surrounding context such that the Planning Board may determine the plan's appropriateness.
- a. Side Yard Treatment
 - i. Where the distance between structures on adjacent lots is ten (10) feet or less the side yard shall be screened by a solid fence, wall or landscape treatment of evergreen plantings at a height not to exceed three (3) feet.
 - ii. Where the distance between structures on adjacent lots is greater than ten (10) feet landscaping shall consist of a combination of materials sufficient to break

up the view into the side yard but, for safety reasons, in no case should this planting be impermeable.

iii. Side yards may, in the alternative, be established as pedestrian walkways to access parking areas to the rear of the building. Such walkways shall be landscaped and lighted for safety.

b. The landscaping plan shall be developed to achieve water conservation through proper plant selection, installation and maintenance practices. The following principles serve as the primary means of achieving water conservation:

- i. Appropriate planning and design
- ii. The use of native, non-invasive, low-maintenance plants
- iii. Limiting turf areas to locations where it provides functional benefits
- iv. Efficient irrigation systems
- v. The use of soil amendments to improve water holding capacity of the soil
- vi. The use of mulches, where appropriate
- vii. The use of drought-tolerant plants
- viii. Appropriate and timely maintenance

c. The use of fertilizers, pesticides, herbicides, and fungicides is prohibited.

5. Lighting. Site lighting, security lighting and architectural/landscape lighting should provide the user with illumination levels appropriate for the designed activity (i.e. parking, walking, outdoor dining) while meeting minimum requirements. Illumination levels should also be reasonably uniform throughout the site and strive to minimize glare. Lighting fixtures shall be of a style appropriate to the character of the district, and be consistent throughout the development. Provide adequate lighting levels in all pedestrian areas, including building entries, along walkways, parking areas, and other public areas. Provide the following in lighting plans:

a. An overlapping pattern of light at a height of about ten (10) to fifteen (15) feet in lighted pedestrian areas and twenty (20) to twenty-four (24) feet in parking areas.



- b. Lighting at consistent lumens with a gradual transition to unlighted areas. Highly contrasting pools of light and dark can be temporarily blinding and should be avoided.
- c. In each lighted area, design lighting levels that will allow pedestrians to identify a face fifteen (15) yards away (generally, a minimum of 4 foot-candles). Adequate lighting reduces anonymity and gives pedestrians an opportunity to choose another route.
- d. Adequate lighting at all building entrances, exits and corridors between buildings, at least four (4) foot candles during active use, especially where doors are recessed.
- e. Confine site lighting to the project site; use shields or other methods to eliminate glare on adjacent properties.
- f. Place light posts and standards so that they do not create hazards for pedestrians or vehicles.
- g. Indicate specific lighting levels in each lighted area.

6. Quality of site furnishings. Provide for the following site plan elements:

- a. High-quality fixtures and materials in site furnishings and features, such as durable and easily maintained walls and paving.
- b. Site features and furnishings that discourage vandalism. Furnishings that are easily removed or do not convey an image of care invite misuse.
- c. Safety materials, such as non-slip walkway surfaces.
- d. Site furnishings shall be of a style appropriate to the character of the district, and be consistent throughout the development.



7. Signs

- a. Sign materials in the Ronkonkoma TOD District for hanging signs: Traditional-looking materials such as wood, brass, bronze, or others are to be used, as they are most appropriate. Wooden signs should be constructed of dense wood that will accept paint readily.
- b. Signs shall be of a style appropriate to the character of the district, and be consistent throughout the development.
- c. Signs shall comply with the requirements of the underlying zone, except that they may be flat or projected from the building a maximum of twenty-four (24) inches, provided they do not conflict with any pedestrian walks or circulation. Additional secondary signs may be permitted by the use of window applied lettering/logos, or interior signs visible through windows at the primary façade at the ground level, where at the Planning Board’s discretion they are found to be consistent with the intent of the regulations.

SECTION 10. ADMINISTRATION

1. The Planning Board shall have jurisdiction to review and approve any development proposals in the Ronkonkoma TOD District submitted pursuant to this Article. The project review and approval process, including applicable time lines and procedures, shall be in accordance with the site plan review process of the Brookhaven Zoning Ordinance (see Sections 85-45 through 85-50.1).
2. Default Procedures. Any issue not addressed herein related to the Ronkonkoma TOD District shall be subject to the requirements contained in the Brookhaven Zoning Ordinance to the extent that such requirements are not inconsistent with the requirements of this Article.